

# SECTION 4.04

## STOCKTON CHANNEL AREA DESIGN GUIDELINES

---

### INTRODUCTION

#### 4.04.010

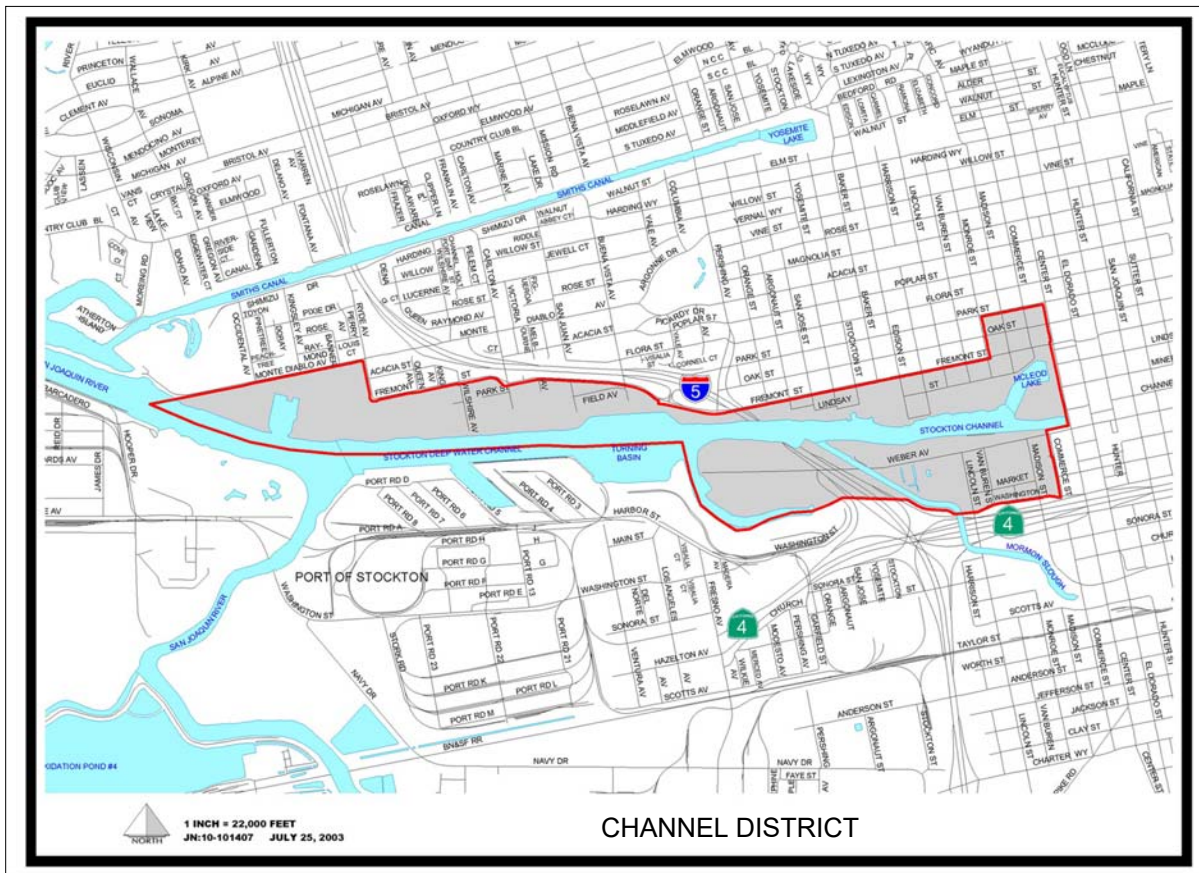


The Channel area is generally defined as the section of the City surrounding the Stockton Channel (see map on following page). A variety of efforts have been undertaken over the years to develop plans for the area, including the *Development Plan for the Stockton Channel* (1974), the *Stockton Waterfront Revival Task Force* (1996), and the *Waterfront Promenade and Marina Development Project* (2001). Many of the recommendations in these plans have been successfully implemented, such as the Weber Point Events Center, Dean DiCarli Waterfront Square, and Waterfront Warehouse retail project.

The success of the waterfront development projects combined with the location appeal of the properties within the Channel area and the significant public investment in a future waterfront promenade system, present a unique opportunity for the integration of public and private uses. The City views the Channel area as one of its greatest assets; as such, it warrants the special design and development consideration found in this section.



The focus of the channel area design guidelines is the primarily underdeveloped North and South Shores of the Channel. The North Shore area separates the active waterfront from the residential/mixed use neighborhood to the north and is visually connected to both Weber Point and the South Shore. The North Shore is envisioned as a mixed use neighborhood that could accommodate multi-family residential, live-work units, multi-use arena, ballpark, hotel, retail shops, restaurants, offices, and recreational uses. The South Shore offers opportunities for mixed use redevelopment and infill projects at the edge of the Channel. The area just to the south of this area is envisioned as a combination of housing, cultural, and recreational uses, and open space.



## APPLICABILITY

### 4.04.020

The special design guidelines in this section apply to new development, redevelopment, and remodeling – commercial, industrial, mixed use, residential, and public facilities – in the North and South Shore areas of the Stockton waterfront. Applicants should also refer to other sections in this manual for basic design guidance not addressed in this special section (e.g. a proposed commercial development should also refer to Section 4.01: General Commercial Design Guidelines, a proposed new multi-family building should also refer to Section 3.03: Multi-Family Residential Developments). The Channel Area is also covered by the Channel Area (-CHA) Overlay District in the Stockton Development Code. Refer to Article 2 for regulations and Article 8 for definitions. The boundaries of the Channel area are shown on the map above.

## GENERAL DESIGN OBJECTIVES

### 4.04.030

The Channel area possesses great potential to serve as a vibrant residential and commercial district in the City of Stockton. To help achieve this goal, the design guidelines provided in the section are based on the following objectives.

- **Waterfront Character** – Maintain a character of development consistent with that of an active waterfront environment. Ensure that new development and redevelopment of existing structures preserves and enhances the maritime character of the Channel area and honors its waterfront heritage. Maintain a high level of architectural design through appropriate detailing, use of quality/durable materials, and the avoidance of blank, uninteresting wall planes.
- **Preserve Significant Structures** – Preserve and enhance historical and other existing structures that contribute to and strengthen the Channel's maritime character, specifically the Stockton Ironworks, Waterfront Warehouse, Sperry Building, Colberg Boat Works, and Stockton Wheel.
- **Waterfront Orientation** – Encourage proposed uses to develop a strong orientation to the waterfront. View the waterfront as the Channel area's "front yard" and place active outdoor pedestrian areas adjacent to the waterfront while also maintaining active, public frontages along adjacent streets.
- **Pedestrian Orientation and Scale** – Establish effective pedestrian linkages between all uses and the waterfront promenade. Limit the height and bulk of buildings adjacent to the waterfront to maintain a sense of pedestrian scale.
- **Protect Views to the Channel** – Enhance visual connections to the waterfront through the provision of view corridors.
- **Mix of Uses** – Encourage a vital mix of residential, retail, entertainment, recreational, cultural, office, and light industrial uses within the Channel area.
- **Quality Development** – Achieve a high level of quality development by ensuring that development fits within the context of the Channel Area, does not negatively impact adjacent uses, provides superior architectural detailing, incorporates appropriate high quality, durable materials, includes significant landscape improvements, and achieves an efficient/aesthetic arrangement of onsite facilities.
- **Compatibility With Surrounding Uses** – Ensure that new development (including redevelopment and remodeling) relate to the waterfront, and complements surrounding uses through appropriate site planning, architectural design, and pedestrian linkages. Ensure that new development is architecturally compatible with the design theme of the Channel Area and is aesthetically pleasing when

viewed from adjacent properties, streets, freeways, and especially the Channel.

- **Functional Site Arrangement** – Ensure that the arrangement of onsite facilities (e.g., buildings, parking areas, accessory uses, etc.) are planned appropriately to establish an efficient, safe, and aesthetically pleasing site layout oriented to the waterfront.
- **Safe/Convenient Circulation and Parking** – Provide safe, convenient, and efficient vehicular access, circulation, parking, loading, and maneuvering. Encourage pedestrian activity by providing convenient access and safe pedestrian routes.
- **Safety** – Maintain a high level of public safety through appropriate design of spaces and amenities, including pedestrian areas, parking lots, landscaping, and lighting.

## SITE PLANNING

4.04.040

### Issues

Site planning for the Channel area considers how the various components of a development (i.e., buildings, circulation, parking, open space, etc.) relate to the waterfront, adjacent streets, and existing development. It also considers how the various components of the project relate to each other within the overall development site. The main issues related to site planning include:

- Ensuring that new development has the appropriate relationship to the waterfront, pedestrian promenade, and street taking into consideration the context of existing surrounding development.
- Ensuring that the arrangement of onsite facilities has been planned in a comprehensive manner and that the layout of the various site components is efficient, convenient, safe, and aesthetic, and promotes a strong pedestrian orientation.
- Ensuring that views to the Channel and waterfront are preserved and enhanced as new development occurs.

### Objectives Supported

- Waterfront orientation
- Pedestrian orientation and scale
- Protect views to the Channel
- Quality development
- Compatibility with surrounding uses
- Functional site arrangement

- Safe/convenient circulation and parking

## A. Building Orientation



1. Buildings should be oriented toward the waterfront and public rights-of-way (i.e., streets and promenades) and placed close to pedestrian movement areas.
2. Buildings should be oriented to make optimal use and views of the waterfront setting. However, facades that face the street should not be ignored and should receive appropriate architectural design detail.
3. Service and storage areas should not be placed adjacent to the street where they may be difficult to screen. Interesting street facades should be maintained.

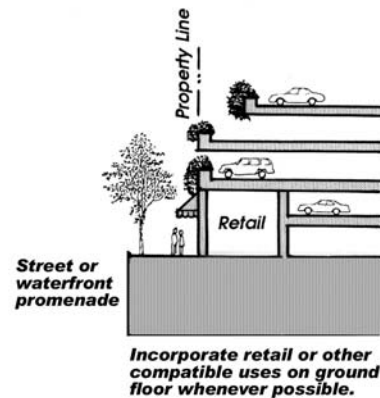


*The provision of outdoor dining areas is strongly encouraged.*

4. Site design that includes pedestrian plazas and courtyards between buildings and between the promenade and buildings are encouraged.
5. Pedestrian connections should be created between the street and water's edge and between the water's edge and parking areas.

## B. Parking and Circulation

1. Parking lots and/or parking structures should not front on the water's edge. The waterfront environment should be preserved and enhanced for the enjoyment of the public.
2. Large parking lots or parking structures that block access of the waterfront are strongly discouraged.
3. Parking lots should be designed to complement their surroundings, from both street level and elevated vantage points (e.g., buildings and freeways) with significant landscaping located throughout the parking lot area.
4. Parking structures should be designed with materials, colors, and architectural articulation that provides compatibility with adjacent buildings and Channel environment.
5. Active, pedestrian-oriented uses at the ground level are encouraged adjacent to streets and waterfront promenade. Ground level parking should not front on these areas. Refer to Section 4.02.090 for additional guidelines regarding parking structures.



## C. Landscaping and Amenities



1. Landscaped open spaces should be provided around all buildings on parcels abutting the waterfront edge to provide opportunities for social activities and views of the waterfront.
2. Landscaped buffers should be provided between residential neighborhoods and more intense commercial and light industrial uses.
3. Pedestrian-scaled lighting, benches, fountains, trees, trash receptacles, and other amenities are encouraged and should conform or be compatible with designs approved by the City for the Stockton Waterfront Promenade.



## ARCHITECTURE

4.04.050

### Issues

The architectural design of buildings in the Channel area must consider many variables from the functional use of the building, to its “fit” within the context of existing development, to its relationship to the waterfront. The main issues related to architectural design in the Channel area include:

- Ensuring that the architectural design of new and renovated buildings promotes a maritime theme.
- Ensuring that existing buildings, which contribute to the Channel's maritime character, are preserved and enhanced.
- Ensuring that the mass and scale of new buildings fit within the context of surrounding development, does not sharply contrast with or dominate other development in the area, and does not block significant views to the Channel.
- Ensuring that the building is well designed by including the appropriate level of design detail on all facades, avoiding blank/uninteresting facades, and providing for the proper screening of equipment and refuse areas, especially adjacent to the waterfront and nearby streets.

### Objectives Supported

- Waterfront character
- Waterfront orientation
- Pedestrian orientation and scale
- Protect views to the Channel
- Quality development
- Compatibility with surrounding uses

### A. Existing Architectural Character

The architectural character of the Channel area is a mix of both industrial and maritime building styles, including materials and design details of brick, corrugated metal, wood, and heavy timbers. New construction and redevelopment should refer to and reinforce the importance of the waterfront along the Channel.

## **B. Building Scale, Massing, and Articulation**

Maintaining the appropriate building scale, massing, and attention to simple details are important to creating a rich and vibrant waterfront environment. This can be accomplished as follows:

1. Incorporate simple modulation of building elevations and roofscapes



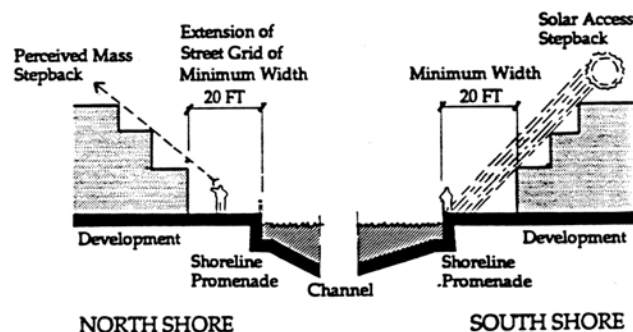


*Desired waterfront architectural character.*

2. Sloped or shed roofs are encouraged where appropriate
3. Architectural treatment should be consistent on all sides visible from the street, pedestrian ways, and the waterfront. There should be no blank walls facing any of these areas.
4. Multiple buildings on a single site should be designed to create strong visual relationships. Waterfront development should take into account the relationship of adjacent buildings in terms of height, materials, scale, and architecture.
5. The size and character of proposed projects should relate to the functions of adjacent streets and pedestrian linkages. Upper stories of buildings should step back from pedestrian areas.
6. New construction should respect the prominence of downtown and downtown structures. New buildings should begin to step down at points immediately adjacent to downtown. Taller, larger-scale buildings may again be appropriate at the western end of the Channel.



7. Height limits should be controlled to allow solar access to existing or proposed parks or open space features. The two locations are south of the Channel Promenade and the sites south of the plaza on the north side of Weber between Center and El Dorado. Solar access is defined as providing sunlight to 50 percent of public open space including the Shoreline Promenade and sidewalk surrounding the park or plaza between 12:00 noon and 1:00 p.m. on the equinoxes.



## C. Building Materials

Building materials that are consistent with the waterfront character of the Channel area are preferred. These include:

- Brick
- Wood
- Masonry
- Metal



## D. Design Details

Encourage design details consistent with a waterfront and conducive to pedestrian activity, such as the following:

1. Use of special materials and unique details (canvas awnings, metal brow canopies, and lights attached to buildings) should be encouraged at a height that defines the first floor.
2. Details reminiscent of waterfront buildings should be encouraged (double-hung wood doors and windows, multi-paned windows, heavy timbers, ghost signs painted directly on building surfaces, external iron staircases, etc.)
3. Roof-mounted equipment should be screened from view from adjacent streets, properties and pedestrian areas. Special attention should be given to buildings whose roofs are viewed from higher elevations. Integrate roof-mounted equipment into the design of the roof.





*Appropriate waterfront design details*