

APPLICABLE DESIGN GUIDELINES

INTRODUCTION 4.00

CONTENT OF CHAPTERS

4.00.010

The Commercial Design Guidelines Chapter contains general design criteria for all commercial uses and structures throughout the City. The chapters also contain special design guidelines for specific types of land uses (e.g., big box retail, shopping centers, service stations, etc.) and for special districts of the City, including downtown, Miracle Mile, and the Channel area.

HOW TO USE THIS CHAPTER

4.00.020

Although this chapter is organized to first provide the general design guidelines for all commercial uses and structures throughout the City, the guidelines in the various sections that make up this chapter should be used as follows:

- A. Project proponents should first determine whether or not their project is located within one of the City's special commercial districts: downtown, Miracle Mile, or Channel area (refer to Sections 4.03, 4.04, and 4.05).
- B. Next, the list of special commercial uses (Section 4.02) should be reviewed to determine if any of the design guidelines for specific commercial uses will apply to the proposed project.
- C. Lastly, Section 4.01 contains General Commercial Design Guidelines, which apply to all commercial projects regardless of the type of use or the location of the proposed project.

If the proposed project is not one of the identified specific commercial uses and is not located in one of the special districts, only the General Commercial Design Guidelines (Section 4.01) will apply. If the proposed project is located in one of the special districts or is an identified specific use, then the project will need to conform to both sets of design criteria—the general guidelines and the specific guidelines, based on location and/or type of use. If there is an apparent overlap between the sets of guidelines, the special district or specific use guidelines shall take precedence.

SECTION 4.01

GENERAL COMMERCIAL DESIGN GUIDELINES

INTRODUCTION

4.01.010

This section provides general design guidelines for all types of commercial development projects throughout the City, including retail, office, and service uses. Other sections within this chapter provide more detailed design guidelines for specific uses (e.g., mixed use, big box retail, vehicle repair) or for specific areas (e.g., downtown, channel area, or areas visible from freeways). In these special cases, the section that addresses the specific use or area should be consulted first. The guidelines in this section will address the less specific, more general design aspects of the project. Both sets of guidelines must be followed to the greatest degree possible.

The design guidelines in this section address:

- Site Planning
- Architecture
- Use of Materials and Colors
- Building Accessories
- Additions, Remodeling, and Rehabilitation
- Landscaping
- Parking and Circulation
- Public Safety Through Design

APPLICABILITY

4.01.020

The design guidelines in this section are applicable to all commercial projects throughout the City, including retail, office, and service uses as follows:

- New commercial development throughout the City, including special commercial districts (e.g., downtown, Miracle Mile, or Channel area) and freeway corridors.

- Additions and exterior remodeling of existing commercial development throughout the City.

GENERAL DESIGN OBJECTIVES

4.01.030

The general commercial design guidelines are based on a variety of specific objectives that establish the basis for the guidelines. The design guidelines in this section are intended to implement the following objectives:

- **Quality Development** – Achieve a high level of quality development by ensuring that development fits within the context of its surroundings, does not negatively impact adjacent uses, provides superior architectural detailing, incorporates appropriate high quality, durable materials, includes significant landscape improvements, and achieves an efficient/aesthetic arrangement of onsite facilities.
- **Consistent Development Pattern** – Maintain a strong sense of continuity along street frontages to strengthen the visual image of commercial corridors.
- **Compatibility With Surrounding Uses** – Ensure that new development (including redevelopment and remodeling) complements surrounding uses and does not create negative impacts for such uses. Ensure that development is aesthetically pleasing, especially when viewed from adjacent properties, streets, and freeways.
- **Functional Site Arrangement** – Ensure that the arrangement of onsite facilities (e.g., buildings, parking areas, accessory uses, etc.) are planned appropriately to establish an efficient, safe, and aesthetically pleasing site layout.
- **Safe/Convenient Circulation and Parking** – Provide safe, convenient, and efficient vehicular access, circulation, parking, loading, and maneuvering. Encourage pedestrian activity by providing convenient access and safe pedestrian routes.
- **Architectural Character** – Maintain a high level of architectural design through appropriate detailing, use of quality/durable materials, and the avoidance of blank, uninteresting wall planes. Provide high quality and visually interesting roof designs consistent with the overall design of the building and surrounding quality development.
- **Landscape Emphasis** – Encourage the extensive use of landscaping in order to achieve visually pleasing development, provide a unified development scheme through a cohesive arrangement of landscape and hardscape elements, provide pedestrian comfort, and enhance views of the site by screening potentially unattractive elements (e.g., trash enclosures, parking areas, etc.).

- **Safety** – Maintain a high level of public safety through appropriate design of spaces and amenities, including pedestrian areas, parking lots, landscaping, and lighting.

SITE PLANNING

4.01.040

Issues

Site planning considers how the various components of a development (i.e., buildings, circulation, parking, open space, etc.) relate to adjacent streets and existing development, and how the various components relate to each other within the development site. The main issues related to site planning include:

- Ensuring the new development has the appropriate relationship to the street given the context of surrounding development.
- Ensuring that new development takes into account its relationship to and interface with surrounding existing development, especially residential uses.
- Ensuring that the arrangement of onsite facilities has been planned in a comprehensive manner and that the layout of the various site components is efficient, convenient, safe, and aesthetically pleasing.

Objectives Supported

- Quality development
- Consistent development pattern
- Compatibility with surrounding uses
- Functional site arrangement
- Safety

A. Determining the Appropriate Development Pattern

The relationship between the location of the on-site buildings, parking areas, circulation routes, open spaces, and landscaping is an important design consideration that must be considered early in the design process. In Stockton, as in most communities of its age and size, there are a number of typical site arrangements that have prevailed over time. These are described in text and graphics on the following pages.

Depending on the requirements of the Development Code and the existing development on adjacent parcels, new infill projects will be expected to follow one of the development patterns described in the following examples in conjunction with the appropriate general design guidelines in this section. To determine which development pattern is the appropriate one to follow, the existing development pattern that occurs on both sides of the street within the block where the project is proposed should be closely observed. From this observation it should be determined which of the four development patterns (i.e., Examples A through D) is most common (occurs most frequently). That is the development pattern that should be followed for the new project, except where the existing development pattern is one that is a poor example and is not appropriate for the area.

If several different development patterns exist and it is difficult to determine which example is the appropriate one to use, the example that creates the most pedestrian friendly environment will usually be the one that is preferred, and should be selected. That is, of the available alternatives, select the development pattern that would place the buildings closest to the street. An exception would be if a particular use suggests a different development pattern and the use of the alternative pattern would not have a negative effect on the general character of the surrounding area.

In a situation where there is no surrounding development from which to determine the existing development pattern, use the design guidelines in Section 4.02 for special commercial uses in conjunction with the appropriate general design guidelines in this section and the requirements of the Development Code.

Example A: Street Adjacent Buildings - Pedestrian Orientation

In this example, buildings are located immediately behind the public sidewalk with no parking between the street edge and the building.



Provide corner "cut-offs" for buildings on prominent intersections.

Place ground level front elevation of the building on the front property line and at the sidewalk edge to maintain the continuity of the "street wall."

Avoid setbacks from the sidewalk edge.

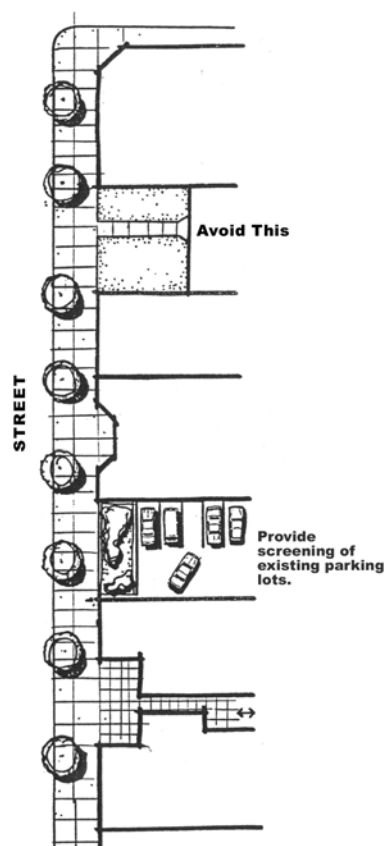
Create continuous pedestrian activity in an uninterrupted sequence by minimizing gaps between buildings.

Avoid parking lots that interrupt a continuous street wall of building frontages.

Avoid blank walls and other "dead" spaces at the ground level.

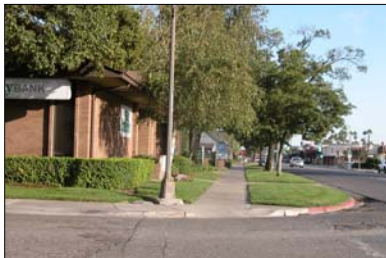
Create pedestrian paseos to parking lots at the rear of buildings.

Use building indentations to create small pedestrian plazas along the street wall.



Example B: Semi-Street Adjacent Buildings - Landscaped Setback

In this example, buildings are set back from the street with a fully landscaped area between the street edge and the building. The setback area is interrupted only by pedestrian areas. No parking occurs in the setback area. Planting and irrigation techniques that promote water conservation (e.g., drought tolerant landscaping) should be incorporated in all landscaped areas.



The setback area should be fully landscaped, interrupted only by pedestrian areas and sidewalks. No parking should be located within any required front yard setback.

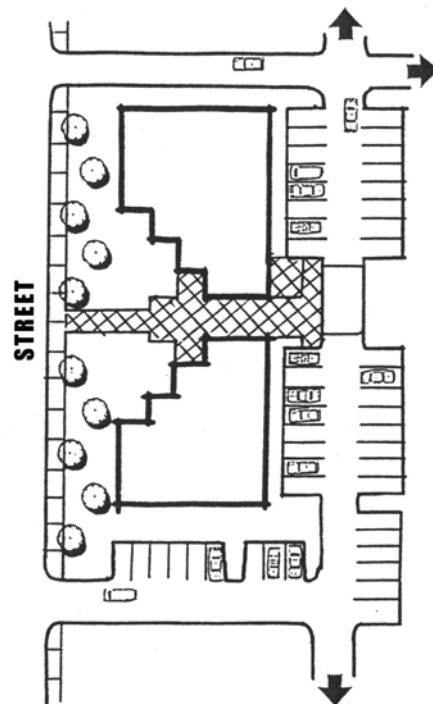
Parking should not be located in the setback space.

Parking at rear of building is preferred.

If parking lots are located at the sides of buildings or elsewhere on the site where they may be visible from the street, they shall be screened from street view by landscaped berms and/or shrubs in compliance with the Development Code.

Driveways should be kept to a minimal number and width as necessary for safety.

Access should be provided to adjoining parcels whenever possible.



Example C: Buildings Set Back - Limited Parking in Front

In this example, limited parking (usually only two parking rows and an aisle) occurs between the street edge and building. Some buildings may be located with a landscaped setback as in Example B. Planting and irrigation techniques that promote water conservation (e.g., drought tolerant landscaping) should be incorporated in all landscaped areas.



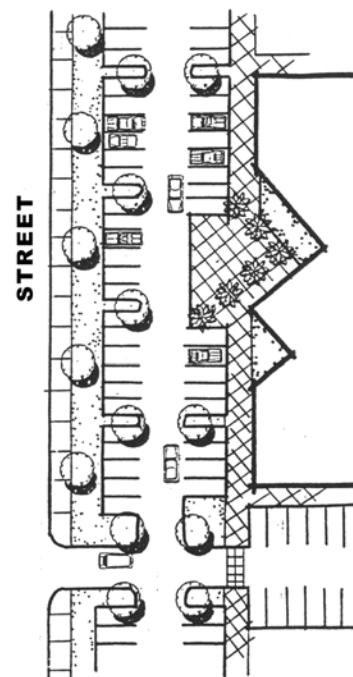
Only one bay of parking (two rows plus aisle) should be provided.

Landscaped pedestrian areas at primary building entrances should be provided.

A combination of trees, shrubs and/or landscaped berms at least 30 inches high should be provided to screen parking areas from view from public rights-of-way.

Accent landscaping and enhanced paving should be provided at project entries.

Vehicular access and pedestrian connections to adjoining parcels should be provided whenever possible.



Example D: Buildings Set Back - Unlimited Parking In Front

In this example, buildings are set back from the street a sufficient distance to provide most of the parking between the street edge and building. Some buildings may be located with a landscaped setback as in Example B. Planting and irrigation techniques that promote water conservation (e.g., drought tolerant landscaping) should be incorporated in all landscaped areas.



Buildings may be setback from street with parking between street and building(s).

Fully landscaped setback at the street edge of all parking areas should be provided in compliance with development regulations.

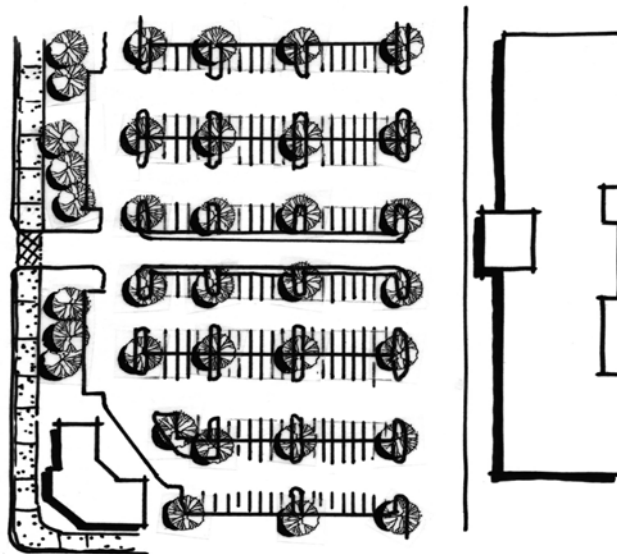
Landscaped pedestrian areas should be provided at primary building entrances.

A combination of trees, shrubs and/or landscaped berms at least 30 inches high should be provided to screen parking areas from public rights-of-way.

Accent landscaping and enhanced paving should be provided at project entries.

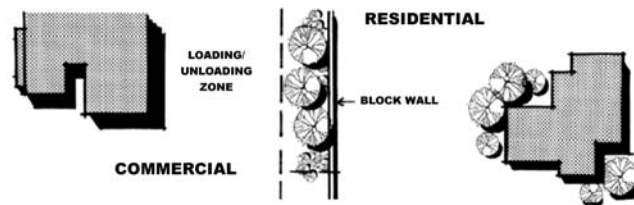
Access to adjoining parcels should be provided whenever possible.

Parking bays should be arranged so that pedestrians walk parallel to drive aisles.



B. Building and Facilities Location

1. The organization of buildings, parking areas, and landscaping should recognize the existing characteristics of the site and should relate to the surrounding development in scale and character.
2. Adjacent residential uses should be buffered from commercial development to the greatest degree possible. Orientation of uses, buildings, and landscaping, and increased setbacks should be used to provide separation between these uses.
3. Commercial development should be oriented away from residential streets. At corner locations, if the side street primarily serves a residential neighborhood, development and access should be oriented away from the side street.
4. Buildings on corner parcels should establish a strong tie to both streets and should encourage pedestrian activity at corner locations.

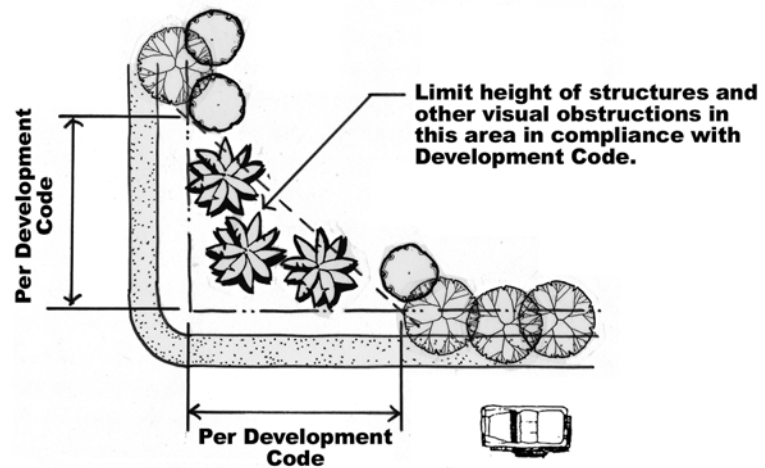


Provide appropriate buffering between incompatible uses

C. Site Access

1. Access to parking lots should be from commercially developed streets. This will help discourage cut through traffic from impacting residential neighborhoods.
2. Site access should promote safety by providing an adequate stacking distance for vehicles between the back of the sidewalk and the first parking stall or circulation aisle.
3. Conflict between vehicles and pedestrians should be avoided at access driveways by providing a sidewalk on at least one side of the driveway.

4. The number of access driveways should be minimized and located as far as possible from street intersections.
5. Site access locations should be coordinated with existing or planned median openings and driveways on the opposite side of the roadway.
6. Unobstructed sight lines at corners and driveways are required in compliance with the Development Code (Traffic Sight Area).



Provide unobstructed lines of sight at corners.

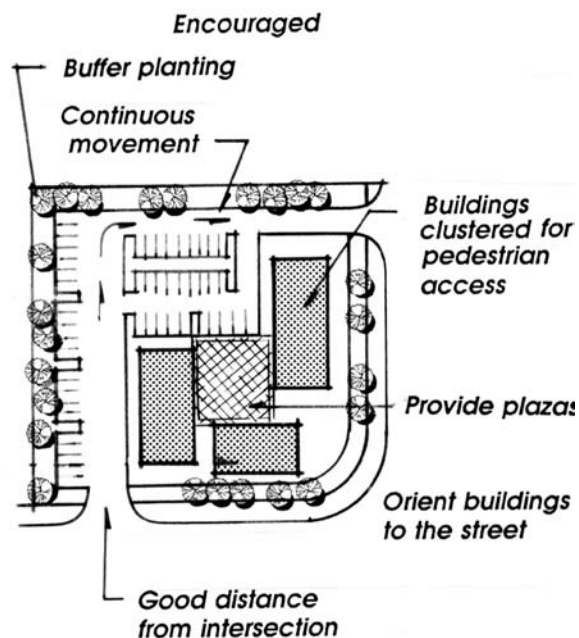
D. Interfaces

1. Adjacent residential and nonresidential uses should be buffered as necessary to maintain a livable residential environment in compliance with requirements of the Development Code (Screening and Buffering). This may be accomplished by the provision of masonry walls, landscaping, berms, building orientation, building height, and limitations on activities adjacent to residential uses.
2. Loading areas, access and circulation driveways, trash enclosure, storage areas, and rooftop equipment should be located as far as possible from adjacent residences and should never be located next to residential properties without fully mitigating their negative effects.
3. Parking lots for commercial uses should have no vehicle access from or to an otherwise predominantly residential street. Pedestrian access from residential neighborhoods to commercial facilities area encouraged.

4. To protect residential privacy and reduce the visual mass of commercial buildings adjoining a residential use, the commercial building should be set back an additional distance in compliance with the Development Code.
5. The orientation of windows in commercial buildings adjacent to residential uses should preclude a direct line of sight into residential properties. Exceptions would be taller buildings in the downtown area.

E. Open Space, Courtyards, and Plazas

1. The organization and design of buildings should encourage and facilitate pedestrian activity.
2. Buildings should be organized to create useable open space, courtyards, plazas, and outdoor dining areas.
3. Convenient, well-defined pedestrian access should be provided from commercial uses to open space, courtyards, and plazas.
4. Pedestrian-oriented open space, courtyards and plazas should include a focal element such as a sculpture and/or water feature and sitting areas.



Organize building to create usable open space.



Open plaza areas create opportunities for outdoor dining.

F. Site Elements

1. Exterior lighting fixtures should be consistent with the architectural theme of the building. All lighting fixtures should be from the same family of fixtures with respect to design, and color of light.
2. Lighting sources shall be shielded to avoid glare in compliance with the Development Code (Light and Glare). To minimize the total number of freestanding light standards, wall mounted lights should be utilized whenever possible
3. Walls visible from public rights-of-way should be decorative and complement the design of on-site buildings. The use of untreated concrete block is discouraged.
4. Visibility from adjacent freeways and on/off ramps should be considered in the placement of trash storage areas. Appropriate screening devices should be provided, including roof structures that screen visibility of the trash enclosure area from above if necessary.
5. Trash enclosures should not be located in areas where they interfere with visibility from vehicles.
6. Trash enclosure areas should be located away from residential uses.

7. Landscaping should be used adjacent to walls and fences to screen flat surfaces.



Use dense landscaping to buffer adjacent uses and screen flat wall.

8. Storage areas should be located in the least visible areas of the site and properly screened in compliance with the Development Code (Solid Waste/Recyclable Materials Storage).



Trash enclosure areas should be appropriately screened with architectural elements and landscaping.

ARCHITECTURAL FORM/DETAILING

4.01.050

Issues

The architectural design of a structure must consider many variables from the functional use of the building, to its aesthetic design, to its “fit” within the context of existing development. The main issues related to architectural design include:

- Ensuring that the mass and scale of the building fits within the context of surrounding development and does not sharply contrast with or dominate other development in the area.
- Ensuring that the building is well designed by including the appropriate level of design detail on all facades, avoiding blank/uninteresting facades, and providing for the proper screening of equipment and trash enclosure areas.

Objectives Supported

- Architectural character
- Quality development
- Consistent development pattern
- Compatibility with surrounding uses

A. Mass and Scale

1. The mass and scale of new infill developments should be compatible with the existing, adjacent structures. This can be accomplished by transitioning from the height of adjacent buildings to the tallest elements of the new (infill) building, stepping back the upper portions of taller buildings, and incorporating human scale elements, such as pedestrian scaled doors, windows, and building materials.
2. Building facades should be detailed in such a way as to make them appear smaller in scale. This can be achieved by articulating the separate floors with horizontal bands or by increasing the level of detail on the building's facade.
3. The size and location of various building elements (e.g., roofs, parapet walls, and wing walls) should not be exaggerated in an attempt to call attention to the building/use or provide additional area or height for signs/advertising.



Without architectural variations buildings appear flat, larger, and “box like”.



Use a variety of architectural elements to create visual interest and reinforce pedestrian scale.

B. Building Facades

1. Design details should be continued or repeated upon all elevations of a building. Details on side and rear views of a building should not be forgotten because of their orientation away from the public right-of-way.
2. Building entrances should be readily identifiable. The use of recesses, projections, columns, and other design elements to articulate entrances are encouraged.
3. Long, blank, unarticulated street-facing facades are strongly discouraged. Facades should be "broken" by vertical and horizontal variations in wall and roof planes, building projections, door and window bays, arcades, and similar elements/techniques.

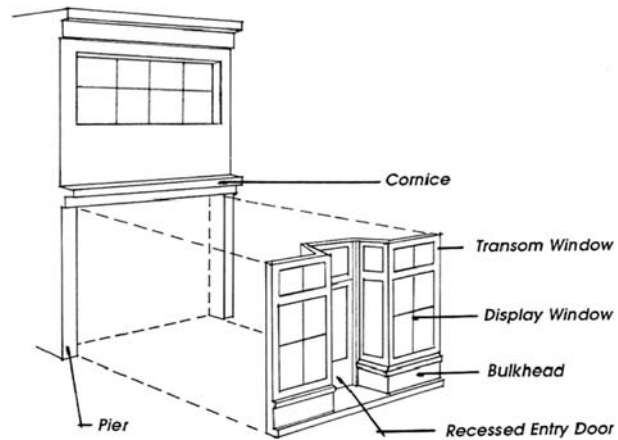


Storefronts should be identified by vertical and horizontal architectural elements.

C. Storefronts

1. Storefronts should be predominantly comprised of transparent surfaces (windows). Storefronts with blank or solid (wall) areas degrade the quality of the pedestrian environment and severely limit visual interest.
2. The use of clear glass (at least 80% light transmission) on the first floor is strongly encouraged. Dark tinted glass and mirror-like films are strongly discouraged.
3. Storefront windows should be large and a minimum of 24 inches off the ground (bulkhead height). The maximum bulkhead height should be approximately 40 inches.
4. Storefront entries should promote a sense of entry into the structure as well as provide a sense of shelter by incorporating elements such as overhangs, canopies, awnings, and recesses.
5. The use of scissor-type security grilles is prohibited since they communicate a message of high crime and cannot be integrated visually into the design of a building.
6. If security grilles are necessary, they should be placed inside the building behind the window display area at a minimum distance of 2 feet behind the window. If this is not physically possible, grilles can be recessed into pockets in the storefront that completely conceal the grilles when they are retracted.

7. Product storage racks should not be placed in such a manner as to block views through storefront windows.



Storefront components.

D. Screening

1. Rooftop or ground mounted equipment should be screened from public streets or any neighboring residential property. Screening devices should be compatible with the architecture, materials, and colors of the building in compliance with the Development Code (Screening and Buffering).
2. Trash enclosures should be located away from residential uses to minimize nuisance for the adjacent property owners in compliance with the Development Code (Solid Waste/Recyclable Materials Storage).
3. Trash enclosures that are visible from the upper stories of adjacent structures or elevated freeways should have an opaque or semi-opaque horizontal cover/screen to mitigate unsightly views. The covering structure should be compatible with the architectural theme of buildings on the site.

MATERIALS AND COLORS

4.01.060

Issues

The proper use of finish materials and colors is very important in the development of a high quality project. The main issues related to the use of finish materials and colors include:

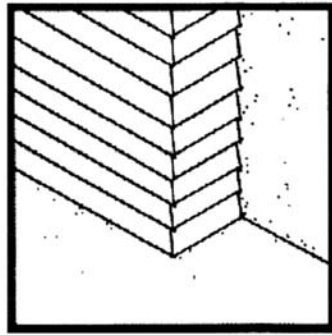
- Ensuring that materials are of a high quality and that they are durable and require minimal maintenance.
- Ensuring that materials are used in a consistent, logical manner that relates to the overall design of the building.

Objectives Supported

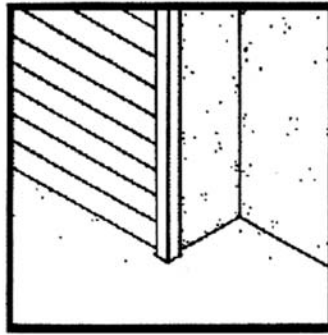
- Quality development
- Compatibility with surrounding uses
- Architectural character

A. Finish Materials

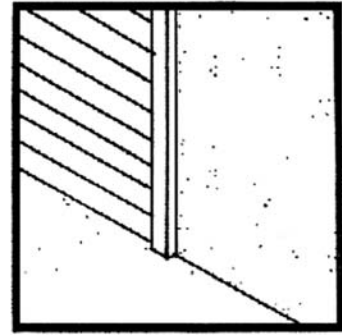
1. Exterior finish materials should be appropriate for an architectural style or theme of the building and should contribute towards a high quality image.
2. Changes in materials should occur at inside corners to make building volumes appear substantial. Material changes at the outside corners or in plane give an impression of thinness and artificiality and should be avoided.



Change in plane with
change in material
Recommended



Material or color change
at outside corner
Not Recommended



Change of materials
on same plane
Not Recommended

3. Materials should be varied to provide architectural interest, however, the number of materials should be limited and not exceed what is required for contrast and accent of architectural features.
4. Exterior materials and architectural details should relate to each other in ways that are traditional and logical. For example, heavy materials should appear to support lighter ones.

B. Color Selection

1. In general, building wall colors should be predominately neutral, off-white, cream, tan, or light pastels. Fluorescent, garish colors shall be avoided.
2. The use of a coordinated three-color palette for the base color and major and minor trim accents is encouraged.

BUILDING ACCESSORIES

4.01.070

Issues

Building accessories (e.g., awnings, lighting, signs, etc.) play an important role in finishing a building's overall design and adding visual interest. The main issue related to building accessories is:

- Ensuring that any accessories added to a building relate to the overall design of the building in an aesthetically pleasing way so that they contribute to a cohesive building design and do not detract from it.

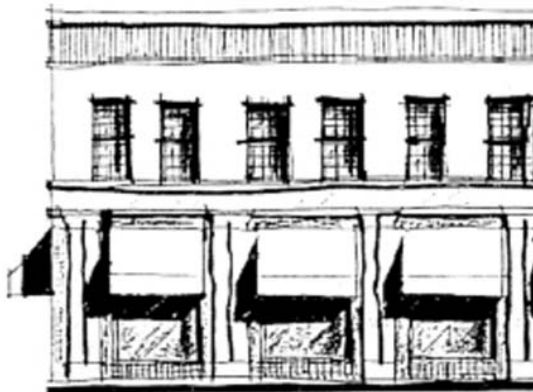
Objectives Supported

- Quality development
- Architectural character

A. Awnings

The use of awnings, canopies, and marquees are encouraged. They provide protection for pedestrians, add interest and color to buildings, and allow placement of pedestrian – oriented signs.

1. Awnings at both the ground level and upper floors should be designed to be compatible with the overall façade of the building and the window and door openings they are associated with. The color of the awnings should be compatible with the rest of the color scheme of the building.
2. Where the façade is divided into distinct bays or sections by vertical architectural elements, awnings should be placed within the elements rather than overlapping them. Awning placement should fit with the scale, proportion, and rhythm created by these elements, and should not cover piers, pilasters, clerestory windows, and other architectural features.



Shed awning is consistent with rectilinear window openings.



Dome-shaped awnings are not appropriate with rectilinear window openings.

3. When there are several businesses in one building, all awnings should be the same in terms of color, trim, and form. Awnings may have business names on the valance to differentiate the individual businesses within the building.
4. Stick-on lettering not designed specifically for adherence to fabric is prohibited.
5. Awnings should be of high quality materials (e.g., canvas, acrylic coated canvas, copper, or glass), shall be fire retardant to meet City standards, and be consistent with the overall building design. Aluminum, vinyl, or backlit awnings generally detract from a quality character and shall not be used.
6. The minimum height of awnings should be 8 feet above the sidewalk and should not project more than 6 feet out from the face of the building. A valance portion of the awning may extend down to not less than 7 feet above the sidewalk.



Awnings may double as signs when properly designed and illuminated.

B. Exterior Lighting

Nighttime illumination is important in creating an interesting and safe environment. In addition, it can serve to highlight building design features, add emphasis to prominent entrances and plazas, and to create an ambiance of vitality and security.

1. Exterior lighting should be designed as part of the overall architectural style of the building. It should relate to the design elements of the building and highlight interesting design features.

2. For safety, identification, and convenience, the entrances of buildings should be well illuminated. The average level of illumination for walkways should be one foot-candle and for security areas, such as building entrances, should be 2 foot-candles.

C. Signs

Refer to Section 6.01, Sign Design Guidelines.

ADDITIONS AND REMODELING

4.01.080

Issues

Adding on to and remodeling existing buildings are means of extending a building's useful life. The main issues to consider when altering a building through these processes include:

- Ensuring that the new addition or remodeled component is consistent with the existing design of the building and not in sharp contrast.
- Ensuring that when buildings are remodeled, especially older ones, that significant design details are maintained and restored if they are important to the character of the building.

Objectives Supported

- Quality development
- Architectural character
- Compatibility with surrounding uses

A. Additions to Existing Structures

1. Additions to existing structures should be designed to be well integrated with the existing structure. The design of the addition should follow the general scale, proportion, massing, roof line, and detailing of the original structure, and not be in sharp contrast.
2. Additions should be interpretations of the existing buildings wherein the main design elements of the existing building are incorporated. This may include: the extension of architectural lines from the existing structure to the addition; repetition of window spacing; uses of harmonizing colors; and the inclusion of similar architectural details (e.g., window/door trim, lighting fixtures, tile/brick decoration).



New additions should complement the existing structure.

3. Building materials used for the addition should be the same or better quality than the existing building. The primary intent is to blend the addition with the existing building while at the same time using high quality, durable materials.
4. If an addition is being proposed to a historic building, the design of the addition should be in compliance with the *Secretary of the Interior's Standards for the Treatment of Historic Properties, 1995, or as amended*.

B. Remodeling and Rehabilitation

1. Buildings are often altered over time in an effort to keep up with changing times or to remake a tired image. These changes often result in a gradual erosion of the original design character of the building. Rehabilitation of buildings that have been inappropriately altered is strongly encouraged.
2. When remodeling is to take place, original materials, details, proportions, as well as patterns of materials and openings should be considered and maintained where appropriate. The use of materials such as cedar shakes, textured plywood/paneling, poor quality fake stone veneer, plastic or corrugated metal paneling, heavy troweled stucco finishes, and similar materials should be avoided.
3. Often in previous remodeling attempts, original decorative details and architectural elements were covered up. In the remodeling process, these forgotten details should be restored and incorporated into the design of the remodeled building.

4. Existing building elements and materials that are incompatible with the original design of the building should be removed. These include inappropriate use of exterior embellishments and modernized elements that are in sharp contrast to the building's original design.
5. If remodeling or rehabilitation of a designated historic building is proposed, all work should be performed in compliance with the *Secretary of Interior's Standards for the Treatment of Historic Properties, 1995*.

LANDSCAPING

4.01.090

Issues

Landscaping has a variety of functions, including softening the hard edges of development, screening unattractive views, buffering incompatible uses, providing shade, and increasing the overall aesthetic appeal of a project. The main issues related to landscaping include:

- Ensuring that the landscape design scheme (including site furniture and paving) is compatible with the overall design of the project in terms of scale, function, and design theme.
- Ensuring that landscape materials are selected for their ability to adapt to Stockton's climate and for their ease of maintenance.

Objectives Supported

- Landscape emphasis
- Quality development
- Functional site arrangement
- Safety

A. Design Concepts

1. Landscaping should help complete the design of the site and not be added as an afterthought. Landscaping should be considered an important design element in the overall plan for any new or redeveloped commercial site.
2. Landscaped areas should generally incorporate planting utilizing a three tiered system: 1) ground covers (including flowering plants—annuals and perennials), 2) shrubs and vines, and 3) trees.



Example of three tier landscape system.

3. Landscaping should enhance the quality of commercial developments by framing and softening the appearance of buildings, screening undesirable views, buffering incompatible uses, and providing shade.
4. Trees located along street frontages should be selected to match or complement existing or proposed street trees in the public right-of-way.
5. A minimum 5-foot net landscape strip should be used along circulation aisles in parking lots, and along building side/rear elevations if a walkway is not used. A landscape strip is encouraged, but not required in nonpublic areas and service areas between pavement and buildings.
6. For office buildings and retail uses, parking should be separated from buildings by landscaped areas and/or raised walkways.



Good example of landscaped setback using a variety of materials.

B. Use of Plant Materials

1. The use of plant material should be well suited to Stockton's climate as required by the Development Code (Landscape Standards).
2. The choice, placement, and scale of plants should relate to the architectural and site design of the project. Plantings should be used to shade and screen, to accent focal points and entries, to complement building design, to break up expanses of paving or walls, and to define on-site circulation.

C. Site Furniture

1. Outdoor furniture and fixtures such as lighting, directional signs, trellises, raised planters, works of art, benches, trash receptacles, phone booths, fencing, etc., should be selected as integral elements of the building and landscape design. These should be included in, and shown on, all site and landscape plans.
2. Outdoor furniture should be of a sturdy construction to withstand daily abuse. Wood should usually be avoided.
3. Outdoor furniture should be located so it will not conflict with the circulation patterns of the site.

4. Outdoor seating should be located so that some will be in shade during the hottest part of summer days and some will be in the sun during the rest of the year.



Benches provide pedestrian comfort and, adjacent trees provide shade.

D. Paving

1. Decorative paving should be incorporated into courtyards, plazas, pedestrian walkways, and crosswalks.



Enhanced paving in pedestrian areas is strongly encouraged.

2. Paving materials should complement the architectural design of the building and landscape design of the development. The use of stamped concrete, stone, brick, pavers, exposed aggregate, or colored concrete is encouraged. The use of slippery materials (e.g., polished marble or granite) is strongly discouraged.
3. The size of areas incorporating decorative paving should be consistent with the function of the area. At driveway entries, the minimum depth from the back of the sidewalk should be 8 feet; however, larger areas may be required.

PARKING AND CIRCULATION

4.01.100

Issues

Onsite parking and circulation often occupy one-half of the site of a commercial project and are highly visible. Their role in the overall design of the site is critical in the development of a safe, efficient project design. The main issues related to parking and circulation include:

- Ensuring that parking and circulation (including access to the site) is laid out in a straightforward, efficient manner that is safe and easy for motorists to understand.
- Ensuring that parking lots do not visually dominate views of the project site and that they are designed, screened, and landscaped to be as aesthetically pleasing as possible.
- Ensuring that loading and delivery areas are integrated into the overall design of the site and located in a manner that does not interfere with other onsite circulation.

Objectives Supported

- Safe/convenient circulation and parking
- Quality development
- Functional site arrangement

A. Vehicle Circulation

1. Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking.

2. Dead-end aisles, even with turnaround areas, are strongly discouraged and should be avoided if possible.

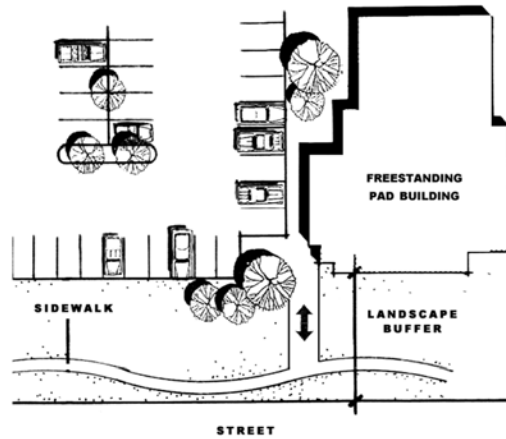
B. Pedestrian Circulation

1. Avoid placing primary vehicle access in close proximity to major building entries in order to minimize pedestrian and vehicular conflicts.
2. Clearly defined pedestrian walkways or paths should be provided from parking areas to primary building entrances. Clear and convenient pedestrian access should be provided between the public sidewalk and the pedestrian areas of the project.
3. Raised walkways, decorative paving, landscaping, and bollards should be used to separate pedestrian paths from vehicular circulation areas to the maximum extent possible.



Pedestrian walkways separated from parking lots are encouraged.

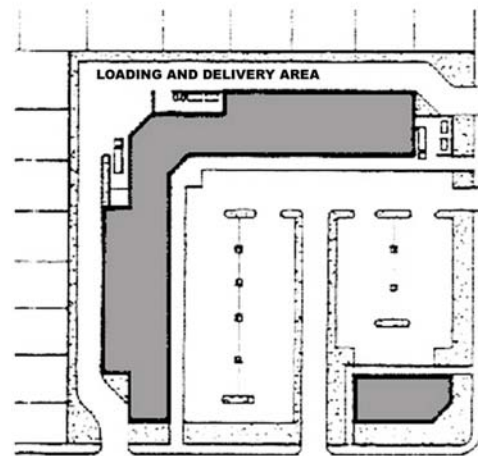
4. Parking areas should be designed so that pedestrians walk parallel to moving cars. Pedestrians should not be required to cross parking aisles and landscape islands to reach building entries.



Provide pedestrian connections between public sidewalk and building(s).

C. Loading and Delivery

1. Loading and delivery service areas should be located and designed to minimize their visibility, circulation conflicts, and adverse noise impacts to the maximum extent feasible.
2. Loading and delivery service areas should be screened with portions of the building, architectural wing walls, freestanding walls, and landscape planting.
3. When commercial buildings back to residential properties, loading areas should be located at the side of the building away from residences whenever possible.
4. To reduce the need for added screening and to decrease the impact on adjacent residential uses, loading areas located inside the building are encouraged.
5. Loading areas should be designed to not interfere with circulation or parking, and to permit trucks to fully maneuver on the property without backing from or onto a public street. Adequate turning areas for ingress/egress to the loading zone should be provided on site.



Loading and delivery areas should be located to the rear of buildings to minimize impacts.

PUBLIC SAFETY

4.01.110

Issues

The promotion of public safety and the prevention of crime through effective design techniques are important aspects to consider in the design of any commercial project. The main issue related to project design for safety and the prevention of crime is:

- Ensuring that strategies and design techniques are incorporated into the design of the project that promote natural surveillance, territorial reinforcement, and natural access control.

Objectives Supported

- Safety
 - Functional site arrangement
 - Safe/convenient circulation and parking
- A. As a security measure, all building entrances should be well lighted. The lighting should be designed so that the lighting is an attractive element in its own right, acting as a public amenity.

- B. Parking lots should be well lighted with one foot-candle of illumination distributed evenly across the parking lot. Entrances to buildings and loading areas should be provided with a minimum of two foot-candles of illumination at ground level.
- C. The design of the outdoor lighting plan should take into consideration the location and potential growth pattern of existing and proposed trees so that appropriate lighting levels are maintained over time.
- D. Window signs should be placed to provide a clear and unobstructed view of the interior of the business establishment from the sidewalk or parking lot.
- E. Entrances to a site and buildings should be designed to be easily visible from a public street, alleyway, or neighboring property. Windows on rear facades that face onto parking lots are very important for helping to deter crime. The use of closed circuit television and "fake" windows should be considered.
- F. Safety behind buildings should be ensured through use of:
 - adequate security lighting
 - limited access controlled by walls, fences, gates, landscaping
 - introduction of activities (e.g., rear entrances for commercial activities) that increase surveillance
 - surveillance through windows or with cameras
 - ongoing maintenance of storage areas and alleys

APPLICABLE GENERAL PLAN POLICY



6

Community Design

The element provides goals, policies, and implementation strategies to address the following design topics:

- General (Section 6.1)
- Traditional Neighborhoods (Section 6.2)
- Commercial Corridors (Section 6.3)
- Districts, Villages, and Neighborhoods (Section 6.4)
- Art in Public Places (Section 6.5)
- Public Works (Section 6.6)
- Design Review (Section 6.7)
- Implementation Measures (Section 6.8)

Key Terms

No unique terms are used in this chapter.

6.1 General

The purpose of the community design element is to establish qualitative urban design goals and policies. The policies are intended to reinforce community-wide concepts depicting a framework of places, districts, corridors, and landmarks. Implementation of the goals and policies support the design review system that encourages unique quality and innovation.

Community design integrates diverse development concepts at an array of levels. From the overall City to neighborhood and districts to streetscape and structure design, the goals and policies presented in this element provide for the visual pattern of land uses and circulation. At the citywide level, these concepts differentiate between the rural outlying areas of the planning area to the central urban core of the Downtown, providing natural transition. Districts and neighborhoods follow, with models for separate, but integrated, areas of the City, each with

its own character. Finally, building and street design standards allow each area of town to insure that its unique appeal is protected.

The policies of the element cover a wide variety of topics from general city design to art. The general policies of the element provide for the overall theme of Stockton as an urban city in the midst of a rural and open space landscape, with emphasis on contrast between the city and outlying areas as well and entry into Stockton. Within the City, policies are utilized that protect the existing traditional neighborhoods, emphasize the design of unique commercial corridors, and encourage public art in both public and private development. In addition, the element focuses on the design of the streets and roadways within the city, seeking to enhance the appearance of Stockton through innovative projects. Lastly, the element provides policies and implementation programs to support design review mechanisms that guide the design concepts, such as guidelines and public review.

CD-1

To improve the overall visual quality of the urban environment.

CD-1.1 Urban Design Plans

The City shall ensure that plans for districts, corridors and villages reflect citywide urban design concepts set out in the General Plan.

CD-1.2 Contrast Between Urban and Rural

The citywide design framework shall heighten the contrast between rural, natural, and urban areas as one enters and travels through the community.

CD-1.3 Travel Experience

To the extent possible, the City shall ensure that all public and private investments in Stockton's districts and villages contribute positively to the overall travel experience by automobile, rail, pedestrian and bicycle in the community.

CD-1.4 Transition to Rural Landscapes

Transitions between urban and rural areas at the edge of the community shall not diminish the visual quality of open space. Soundwalls and utilitarian edges of developments shall not be allowed as an interface between development and rural landscapes.

CD-1.5 Gateways

The City shall define a set of distinctive gateway districts that provide a sense of arrival. Gateway districts shall use a combination of streetscape, building orientation and placement, and signage to create memorable community entries.

**CD-1.6 Open Space Features**

The City shall promote community design that incorporates the open space features of Stockton's waterways, wetlands, and parks into the travel experience. This includes visual access to open space features and private and public investment that visually frames and complements natural landscapes and parks.

CD-1.7 Rail Corridors

The City shall work with transportation agency partners and private property owners to improve maintenance, code enforcement, screening, and landscaping of view sheds along rail transit corridors in Stockton.

CD-1.8 Highway 99 Beautification

The City shall work with Caltrans and private property owners to improve the visual quality of traveling on Highway 99 in Stockton by improving the maintenance of the highway ROW and adjacent properties, code enforcement, reducing the number of billboards, encouraging new investment on visible sites, landscaping, and screening industrial uses.

6.2 Traditional Neighborhoods

Goal CD-2 provides the framework for the protection of Stockton's authentic landmarks and rich historical assets. As a new goal to the General Plan, policies stress the importance of Stockton's assets to sustaining the identity and sense of place within existing communities. New development should be compatible with the existing scale and design of surrounding uses with historical features being preserved and incorporated, as appropriate.

CD-2

To protect historic and authentic qualities of Stockton's traditional neighborhoods and historic districts.

CD-2.1 Protect and Enhance Cultural Resources

To the extent possible, the City shall ensure that new public and private investment protects and enhances Stockton's existing cultural resources, traditional neighborhoods, and historic districts.

CD-2.2 New Infrastructure

The City shall require that new infrastructure investment respect the image and character of historic neighborhoods and districts. Landscape, original roadways, sidewalks and other public realm features in historic neighborhoods shall be restored or repaired where ever possible.

CD-2.3 Incorporate Historic Features

The City shall require new development in districts and villages to incorporate historic and natural features in site and development planning. Rural, agrarian houses and structures of local or historical significance should be preserved and featured in site plans.



CD-2.4 Respect Historic Neighborhood Patterns

The City shall ensure that infill development respects existing historic structures, block and lot patterns, and landscapes. Infill development shall be of compatible scale and character.

6.3 Commercial Corridors

From its earliest years, Stockton has always enjoyed a definitive advantage due to its accessibility by riverboat, railroad, streetcar, and automobile. Goal CD-3 builds upon this “access advantage” through improvement of Stockton’s overall travel experience. New policies to the General Plan include the promotion and implementation of reinvestment projects within Stockton’s older and underutilized commercial corridors through the use of design objectives, signage, and innovative site planning techniques.

CD-3

To improve the image of Stockton’s existing strip districts and corridors.

CD-3.1 Implementation of Design Objectives

The City shall require that renovated and new development contributes to the implementation of the General Plan’s community design objectives and concepts for commercial corridors and districts.

CD-3.2 Each Project Contributes

The City shall require that renovated and new commercial and commercial centers be designed to implement gateways, public spaces and streets, streetscapes, interconnected system of pedestrian ways, or other design features.

CD-3.3 Site Planning

The City shall require that renovated and new commercial buildings and centers be planned and designed so that the location and shape of buildings contribute to the corridor’s identity and urban design concepts. This includes the orientation of buildings, composition of roof forms, and architectural treatments.

CD-3.4 Enhanced Frontage

The City shall require that the frontage of commercial roadways and connecting side streets be enhanced by the design of commercial buildings and centers. They shall improve pedestrian comfort, streetscape image, and building edge continuity.

CD-3.5 Parking and Setbacks

The City shall require that building and parking setbacks be designed as an extension of the urban design concept for the corridor and adjacent neighborhoods. This includes the depth, edge treatment, pedestrian facilities and landscaping of setback areas.

CD-3.6 Support District Open Space Concepts

The City shall require that renovated and new projects support urban design concepts with open spaces that create gateways, act as collectors for pedestrian systems, or provide a social focal point for a project and the surrounding community and corridor.

CD-3.7 Signage

The City shall require that renovated and new commercial buildings and centers have signage and graphic identity concepts that support both project and commercial corridor design objectives.

APPLICABLE DEVELOPMENT CODE

Chapter 16.120 DESIGN REVIEW

16.120.010 Purpose of chapter.

The purpose of this chapter is to establish procedures for the discretionary and nondiscretionary design review of development throughout the City in order to encourage development that is compatible and harmonious with the design and use of surrounding properties and with the City in general. In order to ensure that development in Stockton meets or exceeds the City's expectation of high quality, this design review process has been established to:

- A. Promote the orderly development of the City as provided for in the goals, objectives, and policies of the General Plan.
- B. Preserve and enhance the existing unique character of the City.
- C. Ensure that new or modified development contributes to the positive image of the City by being aesthetically pleasing and functionally organized.
- D. Protect and enhance property values by ensuring that development fits properly within the context of its surroundings and does not negatively impact adjacent uses.
- E. Promote the preservation of the character and architecture of existing neighborhoods, particularly through design review districts.
- F. Provide certainty to the development review process by clearly stating the City's expectations, thereby reducing guesswork and reducing development time and costs.
- G. Encourage individual creativity and innovative solutions by allowing flexibility in how a particular guideline is met as long as the intent of the guideline is achieved.
- H. Ensure high quality development without causing unnecessarily high development costs.
- I. Communicate the desired qualities and characteristics of development which are intended to promote quality design that is sensitive to its neighborhood context, compatible with adjacent land uses and structures, and consistent with the General Plan. (Prior code § 16-515.010)

16.120.020 Applicability.

A. Types of Projects. The following list identifies the types of projects that are subject to the City's design review process.

1. Residential Development.

- a. New single-family tract development in a subdivision of five (5) or more parcels. (These projects are subject to the design standards located in Section 3.01 (Single-Family Design Standards for New Tract Development) of the Citywide Design Guidelines Manual.)
- b. New single-family infill projects located within special districts. (See Section 3.02 of the Citywide Design Guidelines Manual (Single-Family Residential Design Guidelines for Special Districts) for a list of, and a map illustrating, the special districts.)
- c. Additions and exterior remodeling of single-family dwellings located within special districts visible from the public right-of-way.
- d. New multifamily developments containing two (2) or more dwelling units.

- e. Additions and exterior remodeling of existing multifamily projects.
- f. Accessory structures in special districts or as part of a multifamily project greater than 120 square feet that are visible from the public right-of-way.
- 2. **Commercial Development.**
 - a. New commercial development throughout the City, including special commercial districts (e.g., Downtown; Miracle Mile, or Channel area) and freeway corridors.
 - b. Additions and exterior remodeling of existing commercial development throughout the City that is visible from a public right-of-way or an internal circulation route.
 - c. Accessory structures visible from a public right-of-way or an internal circulation route.
- 3. **Business Park and Industrial Development.**
 - a. New business park and light industrial development throughout the City.
 - b. Additions and exterior remodeling of existing business park and industrial development throughout the City that is visible from a public right-of-way or an internal circulation route.
 - c. Accessory structures visible from a public right-of-way or an internal circulation route.
- 4. **Signs.**
 - a. New on-site signs throughout the City.
 - b. New off-site signs throughout the City.

B. Use of Design Guidelines.

- 1. **Adopted Design Guidelines.** The Review Authority shall refer to the City of Stockton Citywide Design Guidelines (Guidelines) and other design guidelines that have been adopted by the Council in order to provide guidance to applicants seeking to comply with the requirements of this chapter.
- 2. **To Be Used by Property Owners and Design Professionals.** The design guidelines are to be used by property owners, architects, designers, developers, and landscape architects in the planning and design of projects in the City.
- 3. **To Be Used as Review Criteria.** The design guidelines are to be used by City staff, the Director, Architectural Review Committee (“ARC”), Cultural Heritage Board, Redevelopment Commission, Commission, and Council as adopted criteria for the review of development proposals subject to the requirements of this chapter. (Ord. 002-06 C.S. § 1; prior code § 16-515.020)

16.120.030 Exemptions.

The following types of projects are exempt from the design review process and are not required to comply with the design guidelines. However, it is strongly recommended that applicants with exempt projects

refer to the design guidelines and follow them to the degree feasible to help ensure well-designed projects.

- A. New single-family dwellings and additions to or remodeling of existing single-family dwellings not located within a special district or in a new subdivision of five (5) or more parcels.
- B. Additions to or remodeling of existing commercial structures when the improvements are not visible from a public right-of-way or an internal circulation route.
- C. Additions to or remodeling of existing business park and industrial structures when the improvements are not visible from a public right-of-way or an internal circulation route.
- D. Accessory structures not visible from a public right-of-way or an internal circulation route.
- E. Interior improvements.
- F. Temporary structures as defined in Division 8 (Glossary).
- G. Routine maintenance of landscaping, signs, and structures. (Prior code § 16-515.030)

16.120.040 Applicable Review Authority.

A. Nondiscretionary Projects. The Director shall be the applicable Review Authority for all nondiscretionary projects, based on the recommendation of the ARC.

B. Discretionary Projects.

1. **Council.** The Council shall be the applicable Review Authority for all discretionary projects authorized by this Development Code for review by the Council, or appealed to the Council in compliance with Chapter 16.100 (Appeals).
2. **Commission.** The Commission shall be the applicable Review Authority for all discretionary projects authorized by this Development Code for review by the Commission, or appealed to the Commission in compliance with Chapter 16.100 (Appeals).
3. **Redevelopment Commission/Agency.**
 - a. The Redevelopment Commission shall be the applicable Review Authority for all discretionary projects authorized by the Municipal Code for review by the Redevelopment Commission.
 - b. The Redevelopment Agency shall be the applicable Review Authority for all discretionary projects authorized by the Municipal Code for review by the Redevelopment Agency.
4. **Director.** The Director, based on the recommendation of the ARC, shall be the applicable Design Review Authority for:
 - a. All certificates of appropriateness authorized by the Development Code, based on the recommendation of the Cultural Heritage Board; and
 - b. All discretionary projects authorized by this Development Code for review by the Director.

(Ord. 015-09 C.S., eff. 12-3-09; Ord. 023-07 C.S. § 108; Ord. 002-06 C.S. § 1; prior code § 16-515.040)

16.120.050 Application filing, processing, and review procedures.

A. Optional Preliminary Design Review. The option of a preliminary design review is strongly encouraged and may be requested by applicants before the formal submittal of an application. This is an informal review by the Director or ARC in order to provide direction/guidance to project proponents before preparation of detailed designs/plans for formal application submittal.

B. Application. The application shall be prepared in compliance with the Department handout for design review, filed with the Department, and accompanied by the fee required by the Council's fee resolution.

C. Initial Review. Each application shall be reviewed by the Director to ensure that the application is consistent with this Development Code, the City of Stockton Citywide Design Guidelines (Guidelines), other design guidelines that may be adopted by the City, and the purpose and intent of this chapter.

D. Procedures.

1. Review.

a. **Nondiscretionary Permits (Building Permit/Allowed Uses).** For projects that do not require any type of discretionary approval, the applicant shall submit design review plans before building permit submittal. The ARC shall review the submitted plans for compliance with the Guidelines, other design guidelines that may be adopted by the City, and/or applicable Development Code standards, and make recommendations and supporting findings in compliance with subsection (D)(2). Following the ARC's review of the plans, the Director shall prepare and transmit a list of the ARC's findings and recommendations to the applicant in a timely manner, noting that a request for reconsideration may be submitted, where applicable.

b. **Discretionary Permit/Entitlement.** For projects that require a discretionary approval of a development application (e.g., use permit, variance, land development permit, planned development permit, etc.), the applicant shall submit design review plans for the proposed project at the same time as the discretionary application is submitted to the Department. The Director shall review the project for compliance with the land use and development regulations contained in this Development Code. The ARC shall review the overall design of the project for compliance with the Guidelines and other design guidelines that may be adopted by the City. In reviewing the design of the project, the ARC shall use the guidelines as a reference, and make recommendations and supporting findings in compliance with subsection (D)(2). Following the ARC's review of the plans, the Director shall prepare and transmit a list of the ARC's findings and recommendations to the applicant in a timely manner, noting that a request for reconsideration may be submitted and/or that the Planning Commission is the final Design Review Authority, where applicable.

2. List of Recommendations.

- a. The list of findings and recommendations shall be forwarded:
 - i. To the applicant within 30 working days from the date the application is deemed complete by the Director.

- ii. To the applicable Review Authority for consideration simultaneously with any accompanying discretionary permit/entitlement application.
- b. Where the recommendations would substantially alter a proposed development, the applicant may submit revised plans with a request for reconsideration by the ARC.
- 3. **Resubmittals.** Following a redesign of, or changes to, a project, the applicant shall resubmit an application for review and recommendation by the ARC. If a recommendation has not been reached by the ARC after two (2) reviews, the project shall be referred directly to the Director for a decision.
- 4. **Determination.** Following submittal of the list of recommendations to the applicant and the submittal of any revisions and request for reconsideration, if applicable, the Review Authority shall review the design of the project in compliance with the list of recommendations in subsection (D)(2), and:
 - a. **Nondiscretionary Permits.** Make a consistency determination.
 - b. **Discretionary Permit/Entitlement.**
 - i. **Director.** If the final Design Review Authority is the Director, the Director shall make a consistency determination.
 - ii. **Other Review Authorities.** If the Director is not the final Design Review Authority, the Director shall make a written recommendation to the Review Authority and the Design Review plans shall be considered with the application(s) by the applicable Review Authority identified in Table 4-1 (Review Authority). Following review of the design review plans, the Review Authority shall make a consistency determination.
- E. **On-Site Review.** The review may include on-site inspection of the subject parcel, if deemed necessary by the ARC or the Director. (Ord. 015-09 C.S., eff. 12-3-09; Ord. 002-06 C.S. § 1; prior code § 16-515.050)

16.120.060 Findings.

The Review Authority shall determine whether a project adequately meets adopted City standards and the Guidelines, based upon consistency with the following findings:

- A. The proposed development is consistent with all applicable provisions of this Development Code and other applicable City ordinances;
- B. The general design considerations, including the character, quality, and scale of design are consistent with the purpose/intent of this chapter and the Guidelines and other design guidelines that may be adopted by the City;
- C. The architectural design of structures and their materials and colors are visually compatible with surrounding development. Design elements (e.g., awnings, exterior lighting, screening of equipment, signs, etc.) have been incorporated into the project to further ensure its compatibility with the character and uses of adjacent development, and/or between the different types of uses in a mixed use development;

D. The location and configuration of structures are compatible with their sites and with surrounding sites and structures and do not unnecessarily block views from other structures or dominate their surroundings;

E. The general landscape design, including the color, coverage, location, size, texture, and type of plant materials, provisions for irrigation, planned maintenance, and protection of landscape elements have been considered to ensure visual relief, to complement structures, and to provide an attractive environment;

F. The design and layout of the proposed project will not interfere with the use and enjoyment of neighboring existing or future development and will not result in vehicular or pedestrian hazards;

G. The building design and related site plans, including on-site parking and loading, has been designed and integrated to ensure the intended use will best serve the potential users or patrons of the site; and

H. Special requirements or standards have been adequately incorporated, when applicable, into the building and/or site design (e.g., American Disabilities Act regulations, historic preservation, mitigation measures, open space, utilities, etc.). (Prior code § 16-515.060)

16.120.070 Conformance to plans.

All work performed under a building permit for which drawings and plans have been approved under the procedures and requirements of this chapter shall conform to the approved drawings and plans. The signed and/or stamped plans shall be submitted before, or at the same time as, the building permit application. (Prior code § 16-515.070)

16.120.080 Post issuance procedures.

Unless otherwise stated below, the requirements for use of property; owner responsibility; issuance of a business license, building permit, or certificate of occupancy; maintenance; and performance guarantees of the project following issuance of a design review approval shall be in compliance with Chapter 16.92 (Implementation Procedures, Conditions, and Requirements). In addition, the following procedures shall apply, as applicable, after issuance of a design review:

A. Appeal. The decision of the Review Authority may be appealed in compliance with Chapter 16.100 (Appeals). The decision of the ARC may be appealed to the Commission, following a request for reconsideration, and the decision of the Commission may be appealed to the Council.

B. Effective Date. A design review approval shall be effective on the date the signed and/or stamped plans are issued by the Director.

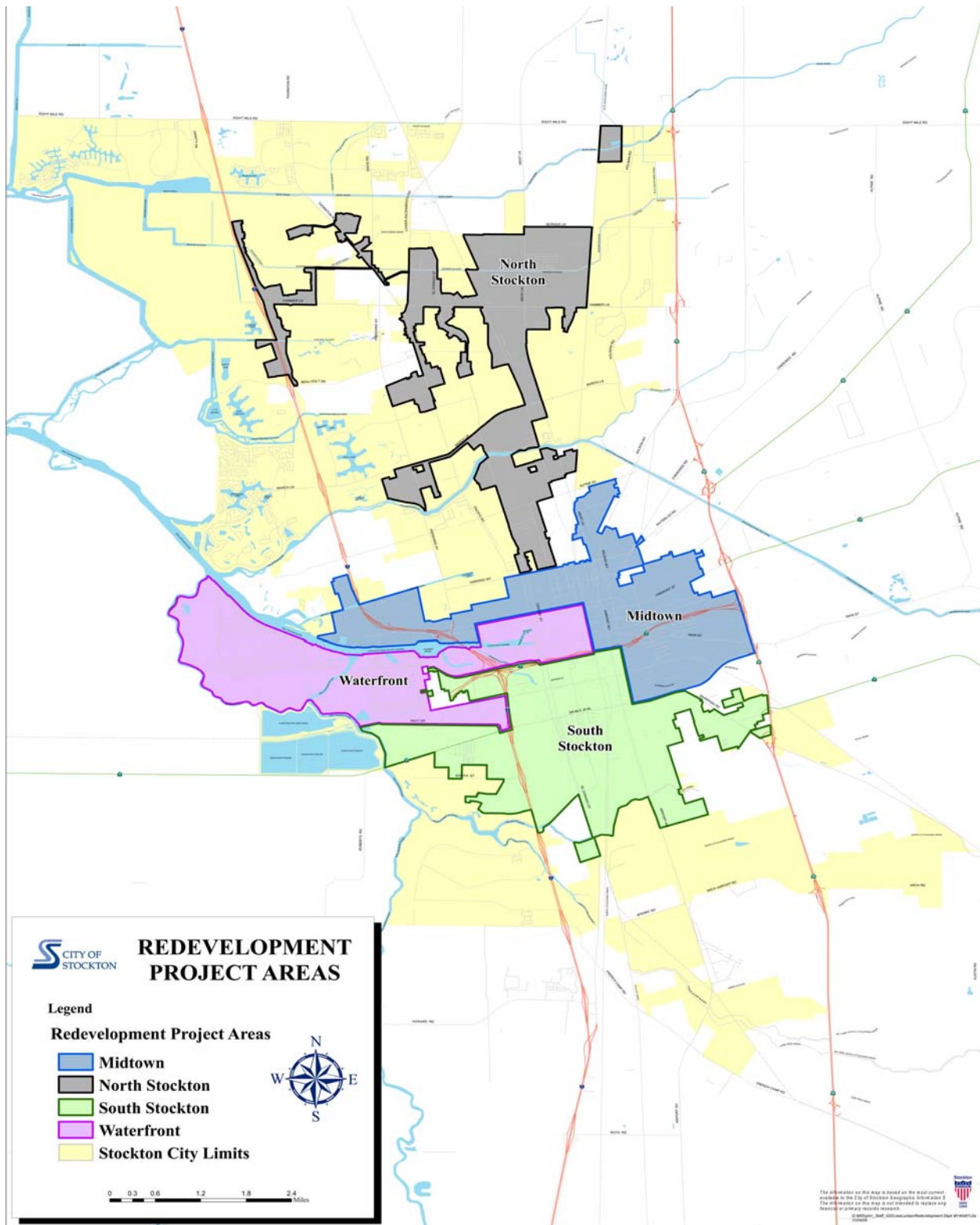
C. Changes to a Design Review Approval. Changes to the project shall be in compliance with Chapter 16.104 (Changes to an Approved Project).

D. Expiration/Extension. The design review approval shall become void unless the required building permit is submitted within 12 months of the design review being issued. An extension may be requested; however, any extension shall be in compliance with the requirements of this

APPLICABLE
WATERFRONT MERGER REDEVELOPMENT PLAN

**STOCKTON REDEVELOPMENT AGENCY
DRAFT IMPELEMENTATION PLAN FOR THE MIDTOWN MERGED PROJECT AREA**

**APPENDIX B-2: STOCKTON REDEVELOPMENT AGENCY
PROJECT AREAS MAP**



REDEVELOPMENT PLAN GOALS

5. The promotion of pedestrian-oriented uses and spaces, and the improvement of safe and convenient pedestrian circulation.
6. The development of a harmonious unified streetscape in commercial areas, including lighting, signage, street trees and furniture, and other design elements, consistent with the applicable design guidelines and standards.
8. The improvement of pedestrian and bicycle circulation between neighborhoods and commercial districts.
17. The enhancement of viable commercial districts, including the attraction and retention of neighborhood serving commercial facilities (such as grocery stores and convenience goods and services stores).